

# Realignment of Beaver Ridge Road

At

Beaver Ridge Road & Oak Ridge Highway

Developed by:  
Ingles Markets Inc.

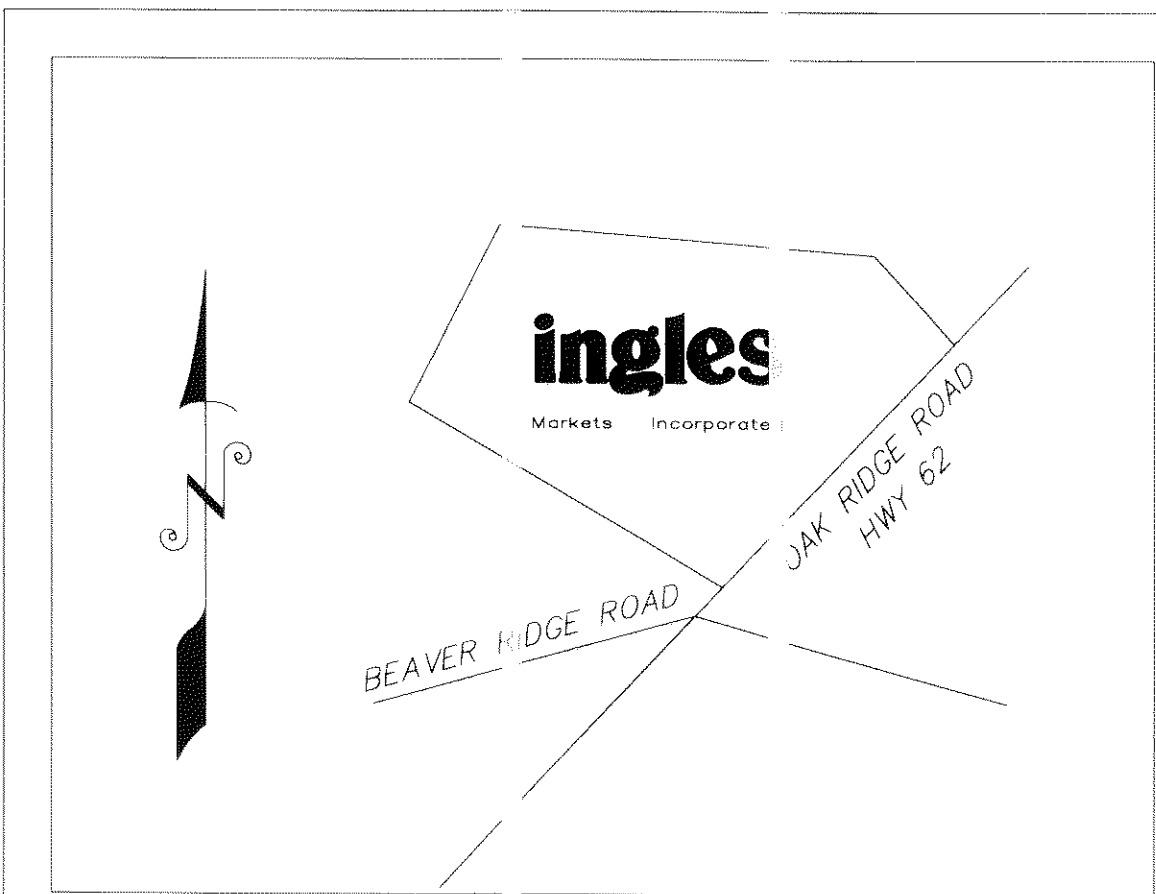
P.O. Box 6676  
Asheville, North Carolina 28816  
(828) 669-2941, Ext. 305  
Attn: Mr. Randy Jameson (VP Real Estate & Distribution)

Intersection of Beaver Ridge Road & Oak Ridge Highway  
Knox County, Tennessee

## DRAWING INDEX

C0.0	Site Cover Sheet	10/15/07	▽
C1.0	Storm Water Pollution Prevention Plan	10/15/07	
C1.1	SWPPP Details and Notes	10/15/07	
C2.0	Site Demolition Plan	10/15/07	▽
C3.0	Site Grading & Drainage Plan	10/15/07	
C3.1	Site Grading & Drainage Details & Notes	10/15/07	
C4.0	Site Layout Plan	10/15/07	▽
C4.1	Site Layout Details	10/15/07	
C5.0	Site Utility Plan	10/15/07	
C5.1	Site Utility Details and Notes	10/15/07	
C6.0	Road Profile	10/15/07	
C7.0	Road Cross Sections Sta. 6+00 to 13+50	10/15/07	
C7.1	Road Cross Sections Sta. 13+50 to 18+00	10/15/07	
C8.0	Traffic Signal Plan	10/15/07	

## REVISIONS



Vicinity Map

NTS

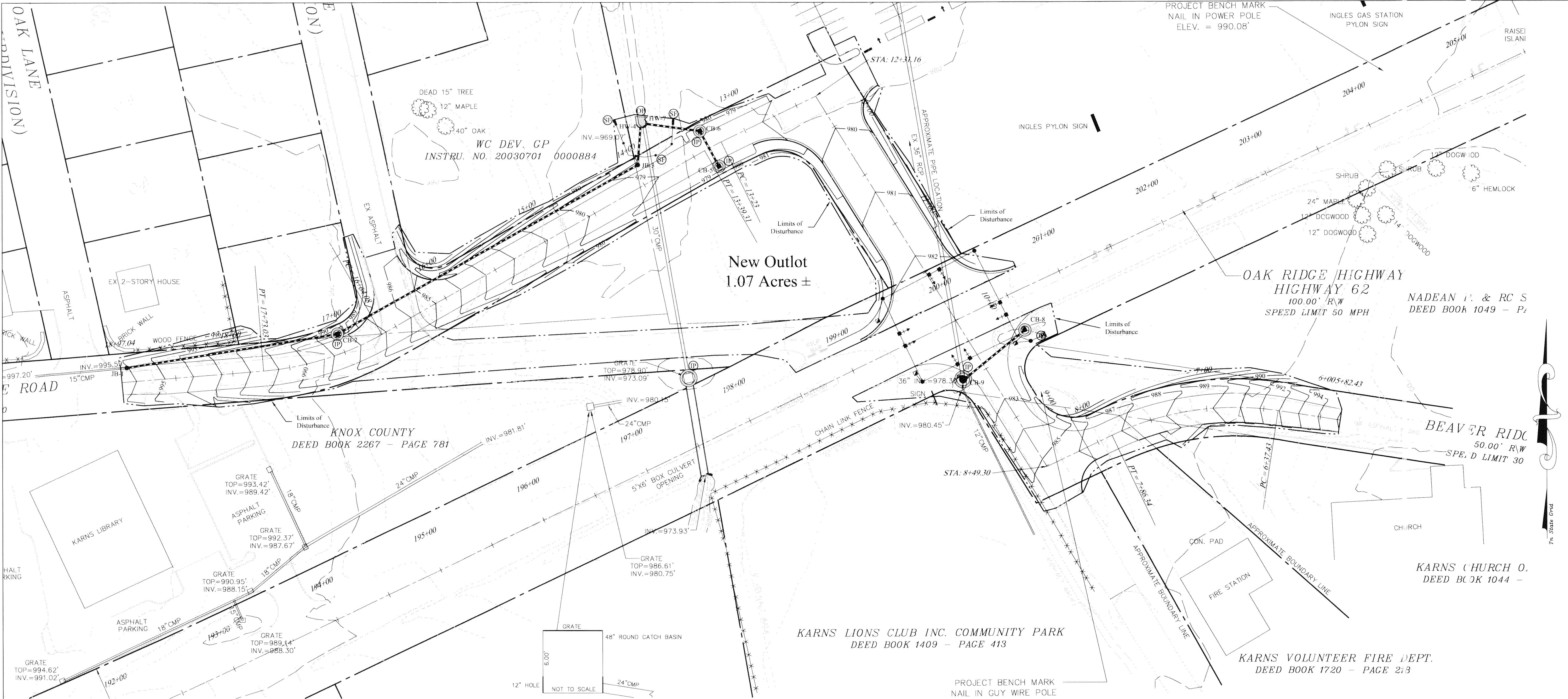
GBS Engineering  
1313 Kalmia Road  
Knoxville, TN 37909  
Phn: 865.566.0185  
Fax: 888.485.7005



REVISED 11/28/07  
12-56-07-C

CONCEPT  
PLAN

REV.	DESCRIPTION	BY	DATE
1	Revised per MPC comments, dated 11/26/07	-	11/28/07
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TITLE: COVER SHEET		DRAWN BY: -	SHEET NO: <b>C0.0</b>
LOCATION: BEAVER RIDGE ROAD KNOXVILLE, TENNESSEE Intersection of Beaver Ridge Rd & Oak Ridge Hwy		CHECKED BY: -	
Owner: INGLES MARKETS INC.		FILE NAME: 1013 C0.0 Sht	
		JOB NUMBER 1013	
		ISSUE DATE: 10/10/0	



INSPECTIONS OF STORM WATER POLLUTION PREVENTION MEASURES

The following inspections shall be conducted before any anticipated storm event, within 24 hours of any storm event of 0.5 inches or greater, and at least twice per week, with a minimum of 72 hours between inspections, until permanent or temporary ground cover is established. Any inspection and associated repairs done within 60 hours before a rain event constitute compliance with "before anticipated storm events", and inspections and repairs on a Friday meet the requirement for rain events over the weekend.

A. Disturbed areas of the construction site that have not been finally stabilized, areas used for storage of materials that are exposed to precipitation, and locations where vehicles enter or exit the site shall be inspected.

B. Disturbed areas and areas used for storage of materials that are exposed to precipitation shall be inspected for evidence of, or the potential for, pollutants entering the drainage system. Erosion and sediment control measures identified in the plan shall be observed to ensure that they are operating correctly.

C. Outfall points shall be inspected to ascertain whether erosion control measures are effective in preventing significant impacts to receiving waters.

Based on the results of the inspection, any inadequate control measures or control measures in disrepair shall be replaced or modified, or repaired as necessary, before the next rain event if possible, but in no case more than seven days after the need is identified. If maintenance prior to the next anticipated storm event is not practical, maintenance must be scheduled and accomplished as soon as possible.

Based on the results of the inspection any modifications to the implemented control measures shall be documented on the storm water pollution prevention plan within 14 calendar days following the inspection.

Inspections shall be documented and include the scope of the inspection, name and title or qualifications of personnel making the inspection, date of the inspection, major observations relating to the implementation of the storm water pollution prevention plan. Any pollutant discharges and any failed or inadequate control device and actions taken shall also be noted.

The permittee shall maintain records of checks and repairs on site.

Records and information resulting from the monitoring activities required by this rule shall be retained for a minimum of (3) three years, or longer if requested by the Division of Water Pollution Control.

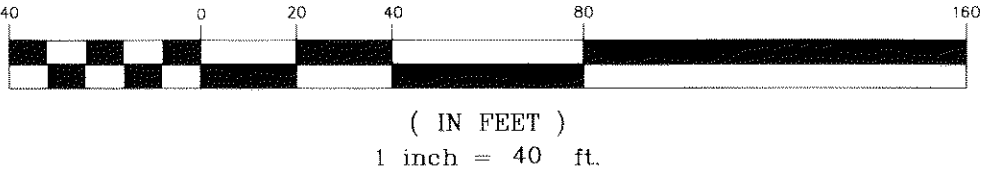
Knowingly making any false statement on any report required by this rule may result in the imposition of criminal penalties as provided for in Section 309 of the Federal Water Pollution Control Act and in Section 69-3-115 of the Tennessee Water Quality Control Act.

LEGEND

- IP Inlet Protection
- CE Temporary Construction Entrance
- OP Outlet Protection - Rip Rap
- SF Silt Fence (Wire Supported)
- DD Diversion Ditch

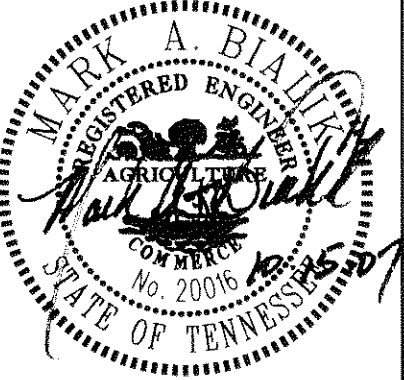
The Owner is responsible for the installation and maintenance of construction pollution prevention controls throughout the life of the project.

GRAPHIC SCALE



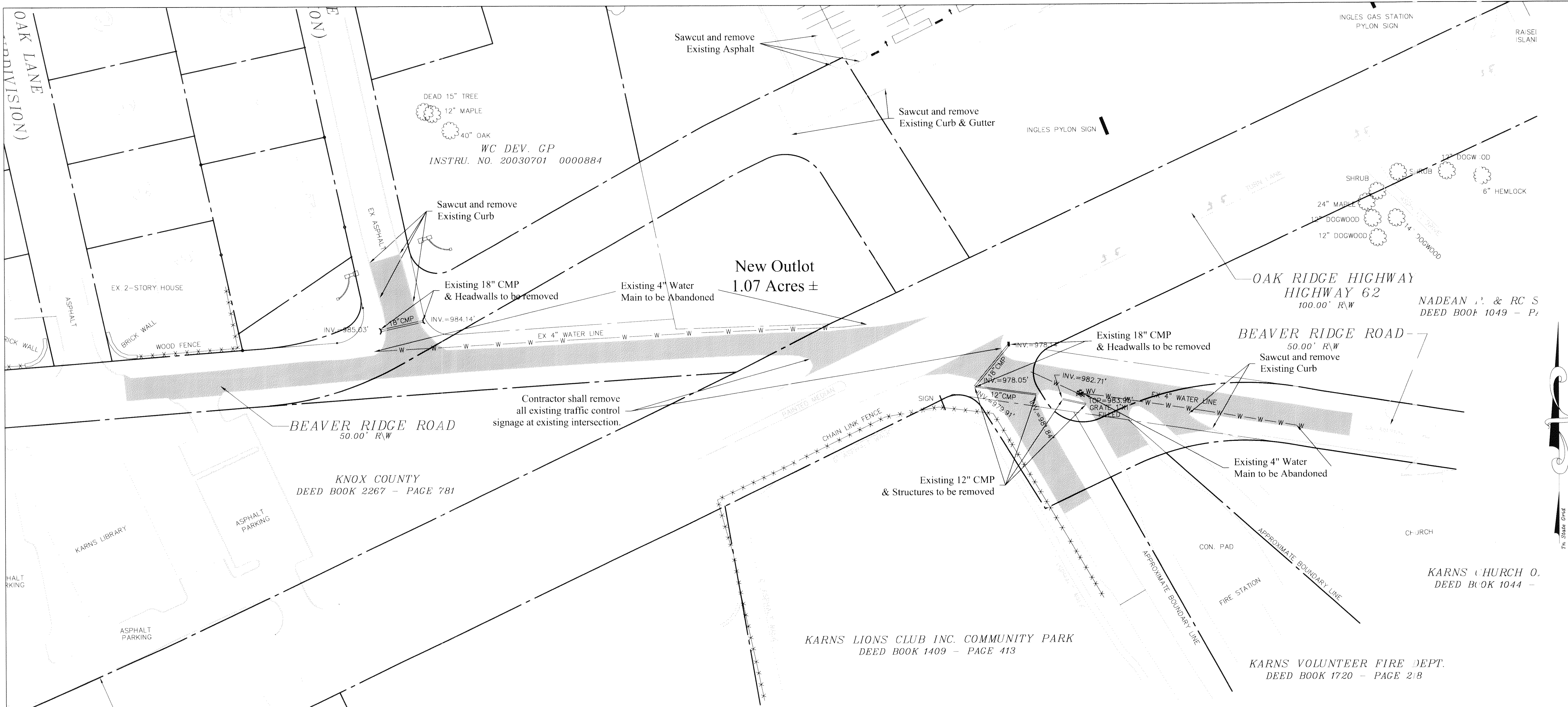
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REV.	DESCRIPTION	BY	DATE
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TITLE: STORM WATER POLLUTION PREVENTION PLAN LOCATION: REALIGNMENT OF BEAVER RIDGE ROAD KNOX COUNTY, TENNESSEE Intersection of Beaver Ridge Rd & Oak Ridge Hwy		DRAWN BY: - CHECKED BY: - FILE NAME: 1013 SWPPP JOB NUMBER: 1013 ISSUE DATE: 10/15/07	SHEET NO: <b>C1.0</b>
Owner: INGLES MARKETS INC.			

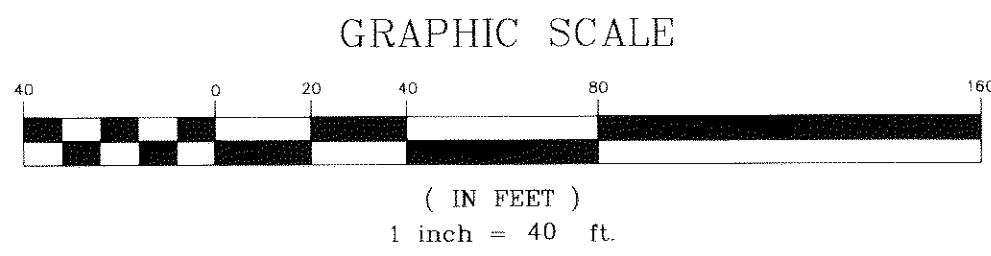




**LEGEND:**

EXISTING ASPHALT PAVEMENT  
TO BE REMOVED

NOTE: ALL EXISTING CURBCUTS  
SHALL BE REATTACHED AS PER  
DRAWING C4.0.



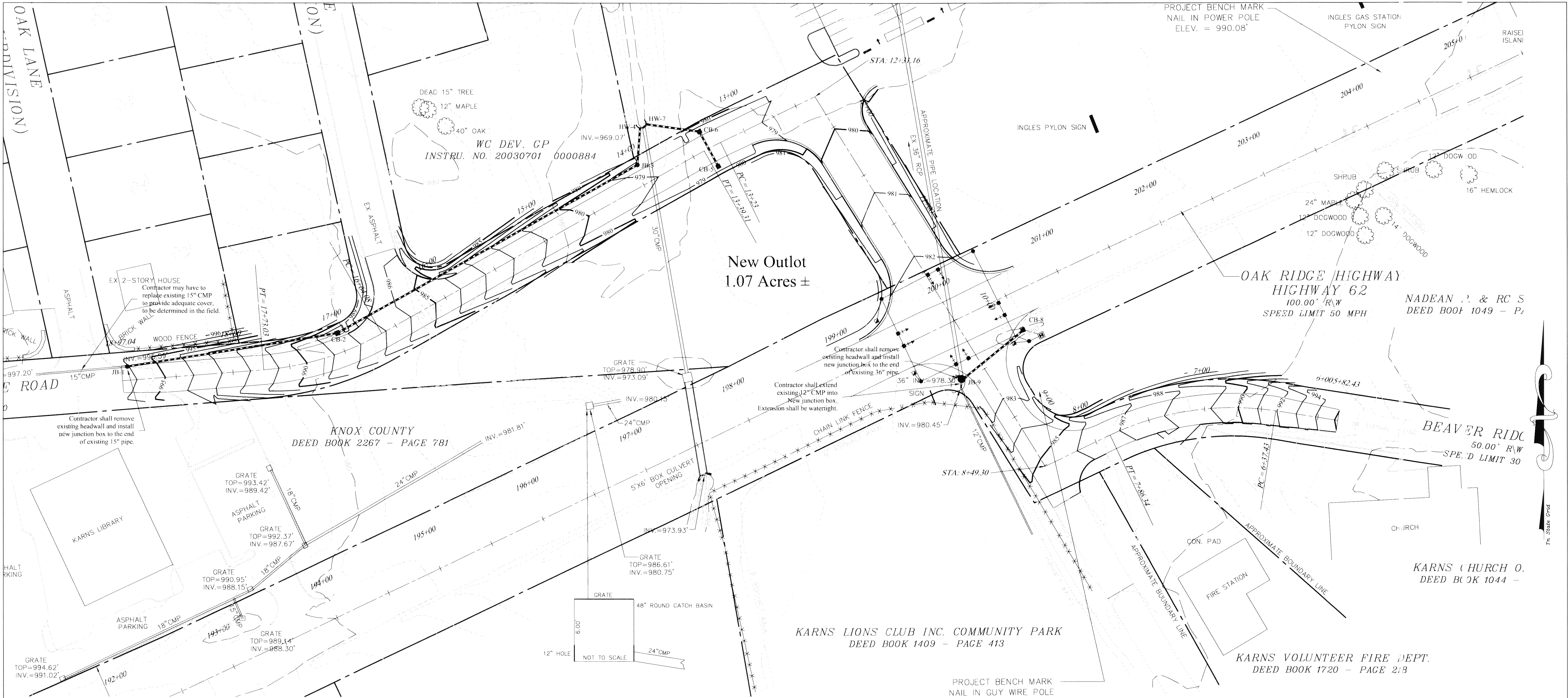
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REV.	DESCRIPTION	BY	DATE
1	Revised per MPC comments, dated 11/26/07	-	11/28/07
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TITLE: SITE DEMOLITION PLAN	DRAWN BY: -	SHEET NO:
LOCATION: REALIGNMENT OF BEAVER RIDGE ROAD KNOX COUNTY, TENNESSEE Intersection of Beaver Ridge Rd & Oak Ridge HWy	CHECKED BY: -	
Owner: INGLES MARKETS INC.	FILE NAME: 1013 Demo	
	JOB NUMBER 1013	
	ISSUE DATE: 10/15/07	

C2.0



**GRASSING SCHEDULE**  
Grass seed shall be as follows:  
FEB-MAY...TALL FESCUE (KY-31) 250 LB/AC  
ANNUAL RYE 50 LB/AC  
AUG-OCT...TALL FESCUE (KY-31) 250 LB/AC  
ANNUAL RYE 50 LB/AC  
SOD:...REBEL II FESCUE (BY LANDSCAPE CONTR)  
  
LIME:...3 TONS/AC  
FERTILIZER: 800 - 1000 LB/AC 10/10/10  
PHOSPHORUS: 500 - 800 LB/AC 20% SUPERPHOSPHATE  
MULCH:...STRAW-1 1/2 TONS/AC (CRIMPED) EROSION CONTROL NET OR MULCH BINDER ON SLOPES.  
  
MIN. SEED PURITY GERM. WEED TALL FESCUE: 90% 90% 1% RYE: 98% 85% 0.1%

- Planting shall be complete on all areas not receive paving or be built upon within 14 working days of completion of grading.
- All lime & fertilizer amounts shown in the phosphorus section above are minimum and shall be verified after soil analysis is accomplished.
- All sod shall be by Landscape Contractor, all other grassing shall be by Grading Contractor.

**STRIPPING AND COMPACTION SCHEDULE**  
Topsoil stripping depth 2"-3" average. Deeper if required locally.  
  
Required compaction percentages (% max dry density)  
Standard Proctor Method ASTM Test # D-698  
Building Area: 98 % Parking Area: 98 %

**TESTING:**  
Allow 1 compaction test per ea. 2500 S.F. (Max 50' x 50' area) per lift at building areas.  
Allow 1 compaction test per each 10,000 S.F. of cut or natural grade at building area.  
Allow 1 compaction test per ea. 5500 S.F. (Max 75' x 75' area) per lift at parking area.  
Allow 1 compaction test per each 40,000 S.F. of cut or natural grade at parking area.

Maximum loose lift thickness shall be 8", unless noted otherwise.

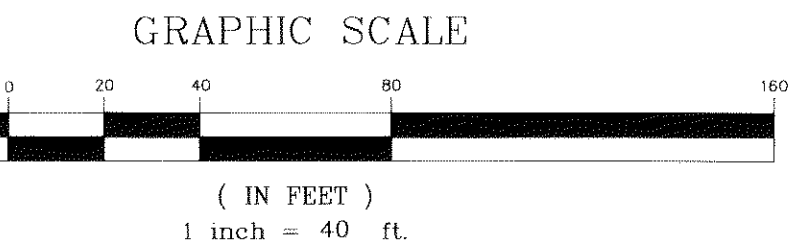
**SITE GRADING, DRAINAGE & EROSION CONTROL DETAIL REFERENCE LEGEND**  
MARK REFERENCED ITEM COMMENT  
HW HEADWALL  
BO BASIN OUTLET STRUCTURE  
CI CURB INLET  
HDPE HIGH DENSITY POLYETHYLENE PIPE  
TC TOP OF CURB  
EP ELEVATION OF PAVEMENT

STORM SEWER PIPE SCHEDULE

PIPE	SIZE (IN)	LENGTH (FT)	SLOPE	UPPER INVERT	LOWER INVERT	REMARKS
JB1 - CB2	18	186	4.03 %	992.00	984.50	--
CB2 - JB3	18	301	3.45 %	984.40	974.00	--
JB3 - HW4	18	33	5.81 %	973.90	972.00	--
CB5 - CB6	18	35	0.71 %	974.00	973.75	--
CB6 - HW7	18	47	3.53 %	973.65	972.00	--
CB8 - JB9	18	69	0.29 %	978.50	978.30	--

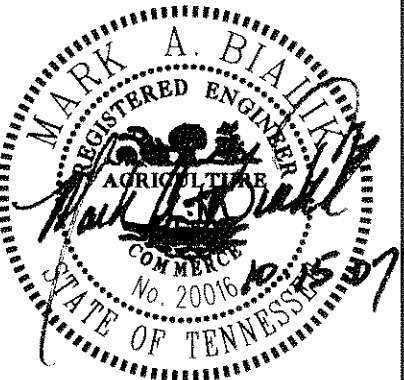
STORM SEWER STRUCTURE SCHEDULE

STRUCTURE	RIM ELEV	PIPE INVERTS		BOTTOM ELEV	SIZE	REMARKS
		OUT	IN			
JB - 1	996.00	992.00	N/A	992.00	48" MIN	TDOT STD DWG D-MH-2
CB - 2	988.50	984.40	984.50	984.40	48" MIN	TDOT STD DWG D-CB-38RB
JB - 3	978.00	973.90	974.00	973.90	48" MIN	TDOT STD DWG D-MH-2
CB - 5	978.00	974.00	N/A	974.00	48" MIN	TDOT STD DWG C-DB-38RB
CB - 6	978.00	973.65	973.75	973.65	48" MIN	SEE DETAIL FOR BASIN RISER
CB - 8	982.00	978.50	N/A	978.50	48" MIN	TDOT STD DRG D-CB-38RB
CB - 9	982.00	978.30	978.30	978.30	84" MIN	TDOT STD DRG D-CB-39RB



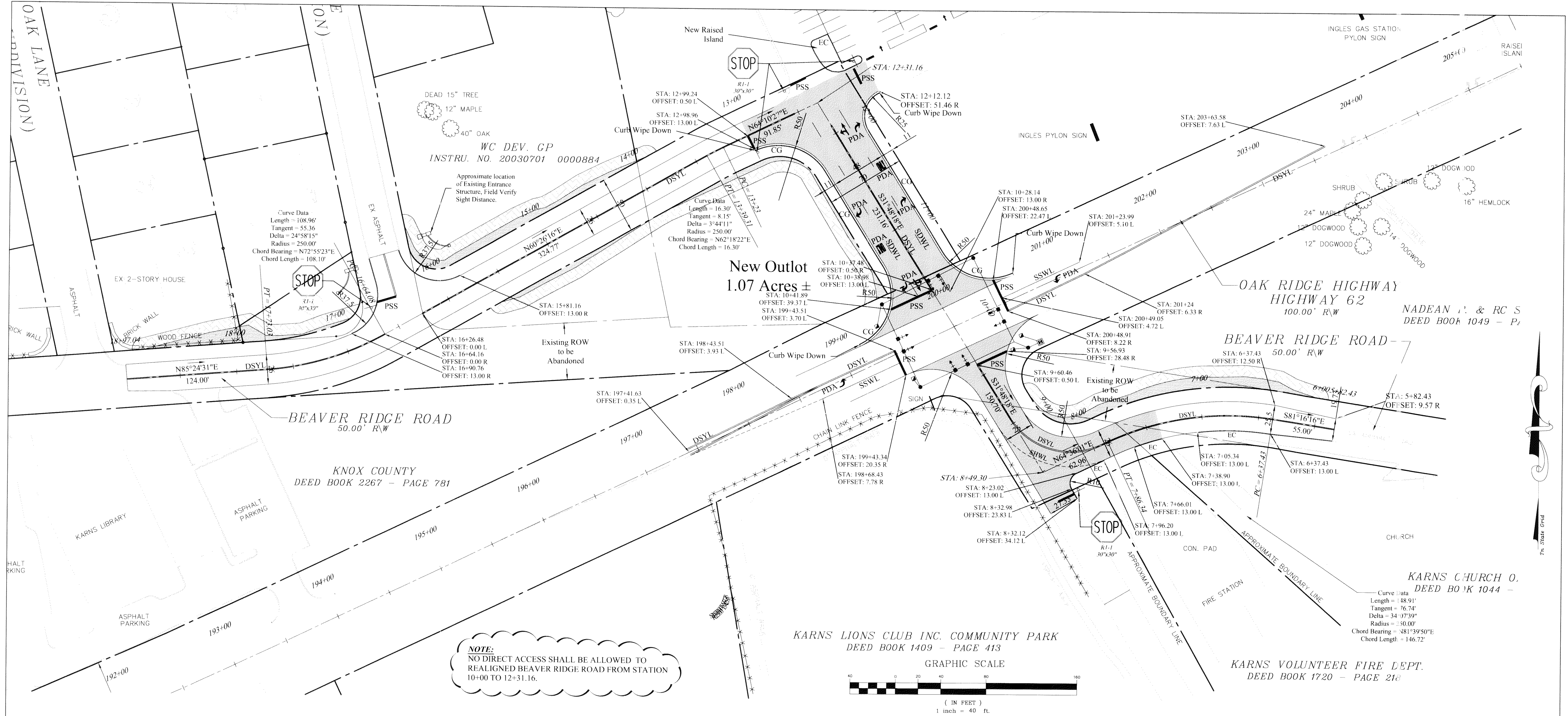
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REV.	DESCRIPTION	BY	DATE
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-	--	-	-
TITLE: SITE GRADING & DRAINAGE PLAN		DRAWN BY: -	SHEET NO:
LOCATION: REALIGNMENT OF BEAVER RIDGE ROAD KNOX COUNTY, TENNESSEE Intersection of Beaver Ridge Rd & Oak Ridge Hwy		CHECKED BY: -	C3.0
Owner: INGLES MARKETS INC.		FILE NAME: 1013 Grade	
		JOB NUMBER 1013	
		ISSUE DATE: 10/15/17	





**SITE PAVING NOTES**

**PAVING ELEVATION TOLERANCE**  
Final subgrade shall be inspected by paving contractor for required elevation and adequate compaction before commencing any paving activity. Paving elevation shall not have more than 1/4" deviation in any 10' increment.

**PAVING THICKNESS TOLERANCE**  
Paving thickness at any one core shall not be less than 90% of specified thickness. The average of all approved tests must be at least equal to the specified thickness for each material. Not more than 20% of all approved test may fall below specified minimums. Any area found to be less than 90% specified thickness shall be overlain with asphalt as directed by GBS Engineering.

**TESTING DURING PLACEMENT**  
General contractor shall perform several spot checks of paving base thickness prior to installation of any asphaltic or concrete paving. Density tests shall be performed on each lift at the rate of one test per 10,000 square feet.

Contractor shall provide for testing of completed paving installation for compliance with design criteria and required thickness. Contractor shall replace all non-conforming material at his own expense.

Testing shall be performed only by an independent testing agent approved by GBS Engineering. Test holes not spotted by GBS Engineering may not be considered acceptable. Allow a minimum of sixteen core tests to be located only by written instruction by GBS Engineering.

The paving contractor shall repair all coring holes by filling holes with concrete flush to top of pavement.

**ASPHALTIC PAVING MATERIALS**  
Material and installation shall be in accordance with the latest edition of the specification of the highway department of the state in which the project is located. Refer to Paving Materials Schedule.

**CONCRETE**  
Provide tooled expansion joints @ 20' O.C. (Max) and expansion joints @ 50' O.C.

Concrete curbing shall be 4000 PSI 28 day strength 2" - 3" slump, (no curb deformation) 3% - 5% air entrained. Install with RC-70 tack coat emulsion over asphalt. Provide cylinder break test - 3/500 L.F. of curbing or 1-day pour with breaks - 1 @ 7-days, 2 @ 28-days. Comply with ASTM C-31 and ASTM C-39. Also provide one slump test per 500 L.F. of curbing or 1-day pour.

Refer to Project Manual for additional requirements.

**PAVEMENT MARKING PAINT**  
Provide pavement marking as shown on the drawings. All pavement markings shall be reflective Thermoplastic.

**SITE PAVING DETAIL REFERENCE SCHEDULE**

MARK	REFERENCED ITEM	COMMENT
CG	CONCRETE CURB & GUTTER	
EC	EXTRUDED CURB	
PDA	PAINT DIRECTIONAL ARROW	THERMOPLASTIC
SDWS	SOLID DASHED WHITE STRIPING	EACH DASH 4" WIDE
DSYL	DOUBLE SOLID YELLOW LINE	EACH LINE 4" WIDE
SSWL	SINGLE SOLID WHITE LINE	EACH LINE 4" WIDE
SHWL	SINGLE CHICKEN HATCH WHITE LINE	EACH LINE 4" WIDE
PSS	STOP BAR	STOP BAR 2' WIDE, THERMOPLASTIC

**SITE LAYOUT NOTES**

In the preparation of these site drawings, GBS Engineering has relied solely on an owner furnished survey and as such GBS Engineering does not assume responsibility for correctness of that information.

**PROPERTY LINE STAKEOUT**  
All property lines shall be marked for rough location with continuous surveyor flagging tape.

All property corners and all outlot corners shall be rough pinned with 2" high metal rod or equivalent and flagged with surveyor tape at completion of rough grading and maintained throughout course of work.

**BUILDING STAKEOUT**  
Building dimensions shown on-site plan are nominal dimensions only intended for use in determine approximate square footage. See foundation plan for actual dimensions of construction.

A surveyor licensed in the state of the project location must be used in locating all buildings on-site before work is begun on footings of any buildings. GBS Engineering shall be notified by the surveyor upon completion of this work.

**SITE WORK STAKEOUT**  
Consult GBS Engineering for all dimensions and other items labeled "verify" or "field verify" prior to installation of that work or any related work.

**LEGEND:**

COLLECTOR STREET ASPHALT PAVEMENT

LOCAL STREET ASPHALT PAVEMENT

CONSTRUCTION EASEMENT (10' WIDE)

SLOPE EASEMENT

**PAVING MATERIALS SCHEDULE**

	THICKNESS	TDOT SPECIFICATION
BASE LIFT	8"	TYPE A, GRADE D AGGREGATE
ASPHALTIC BINDER	2.5"	TYPE B BINDER MIX
ASPHALTIC TOPPING	1.5"	TYPE D SURFACE MIX

**LOCAL STREET PAVING**

PRIME COAT	0.25 GAL PER SQ. YD. OVER TOP BASE LIFT
TACK COAT	0.05 - 0.10 GAL PER SQ. YD.

**COLLECTOR STREET PAVING**

BASE LIFT	10"	TYPE A, GRADE D AGGREGATE
ASPHALTIC BINDER	3.5"	TYPE B BINDER MIX
ASPHALTIC TOPPING	1.5"	TYPE D SURFACE MIX
PRIME COAT	0.25 GAL PER SQ. YD. OVER TOP BASE LIFT	
TACK COAT	0.05 - 0.10 GAL PER SQ. YD.	



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REVISED  
11-28-07

REV.	DESCRIPTION	BY	DATE
1	Revised per MPC comments, dated 11/26/07	-	11/28/07
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TITLE: SITE LAYOUT PLAN		DRAWN BY: -	SHEET NO: <b>C4.0</b>
LOCATION: REALIGNMENT OF BEAVER RIDGE ROAD KNOX COUNTY, TENNESSEE Intersection of Beaver Ridge Rd & Oak Ridge Hwy		CHECKED BY: -	
Owner: INGLES MARKETS INC.		FILE NAME: 1013 Layout	
		JOB NUMBER 1013	
		ISSUE DATE: 10/15/07	



GENERAL NOTES:

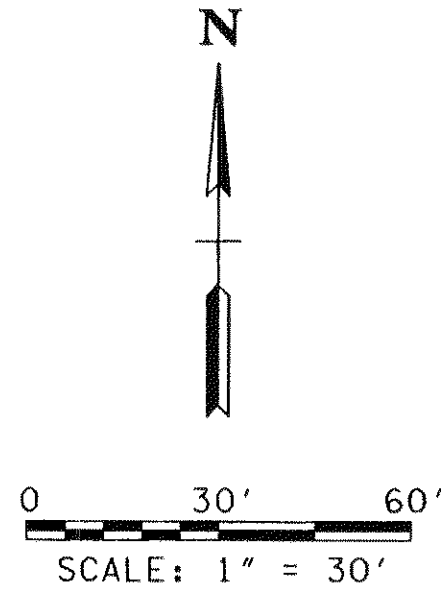
1. EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH THE APPLICABLE SECTIONS OF THE TDDT STANDARD SPECIFICATIONS SECTION 730, "SPECIAL PROVISIONS REGARDING SECTION 730K-TRAFFIC SIGNALS", TDDT STANDARD ROADWAY AND STRUCTURE DRAWINGS (SHEETS T-SG-1 THRU T-SG-13), AND THE REQUIREMENTS OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. THE SIGNAL CONTROLLER SHALL BE A PEEK TRANSYT 3000E, WITH 8 PHASE PAD MOUNTED CABINET, COMPLETE WITH PANELS, WIRING, AND OTHER INCIDENTALS REQUIRED FOR FUTURE CLOSED LOOP OPERATION.
3. THE CONTRACTOR SHALL NOTIFY CANNON & CANNON, INC. (ALAN CHILDERS, 865-670-8555) OF THE DATE AND TIME OF THE PROJECT FINAL INSPECTION. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE DAYS PRIOR TO THE INSPECTION.
4. ANY DISTURBED AREA DURING CONSTRUCTION SHALL BE RESTORED TO ORIGINAL CONDITION OR AS ACCEPTABLE BY KNOX COUNTY.
5. ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.
6. THE SIGNAL SHALL FLASH FOR A MINIMUM OF SEVEN DAYS AND A MAXIMUM OF FOURTEEN DAYS PRIOR TO FULL ACTIVATION.
7. SIGNS W3-3 WITH FLAGS AND "NEW" PLATE SHALL BE INSTALLED ON OAK RIDGE HIGHWAY 500 FT. IN ADVANCE OF THE STOP BAR AND ON BEAVER RIDGE ROAD 150 FT. IN ADVANCE OF THE STOP BAR. TEMPORARY SIGNAL ASSEMBLIES SHALL BE INSTALLED PRIOR TO SIGNAL ACTIVATION, AND THE FLAGS AND "NEW" PLATES SHALL BE REMOVED AFTER EIGHT WEEKS. SIGN FACES SHALL BE MINIMUM HIGH INTENSITY GRADE, AND ALL SIGN MATERIALS AND INSTALLATION SHALL BE IN ACCORDANCE WITH SECTION 713 OF THE TDDT STANDARD SPECIFICATIONS.
8. ALL LED INDICATIONS SHALL MEET THE CURRENT MINIMUM STANDARDS PUBLISHED BY THE INSTITUTE OF TRANSPORTATION ENGINEERS. INCANDESCENT OR SCREW-IN MODULES ARE NOT ACCEPTABLE AND COMPATIBILITY WITH CONFLICT MONITORS AND LOAD SWITCHES SHALL BE TESTED AND CONFIRMED. MANUFACTURER SHALL PROVIDE A MINIMUM FIVE YEAR WARRANTY FOR OPERATION OF THE UNIT.
9. CONTRACTOR SHALL REMOVE EXISTING STOP SIGNS ON BEAVER RIDGE ROAD AT OAK RIDGE HIGHWAY IMMEDIATELY AFTER THE SIGNAL IS MADE FULLY OPERATIONAL.

## INITIAL CONTROLLER TIMING

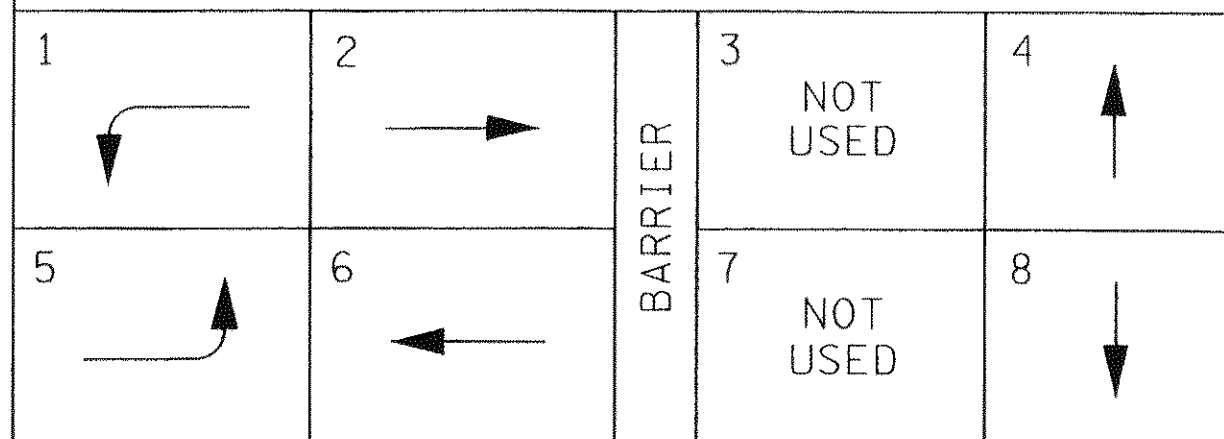
TIMING PHASE	INTERVAL								
	INIT.	PASS.	YELL.	RED CLR.	MAX 1	MAX 2	PED WALK	PED CLR.	RECAL.
1	5.0	2.5	4.0	1.0	10	10			
2	25.0	2.0	4.5	1.5	50	50			MIN.
4	8.0	3.0	4.0	1.5	15	15			
5	5.0	2.5	4.0	1.0	10	10			
6	25.0	2.0	4.5	1.5	50	50			MIN.
8	8.0	3.0	4.0	1.5	15	15			

SPECIAL PROGRAMMING AND NOTES:

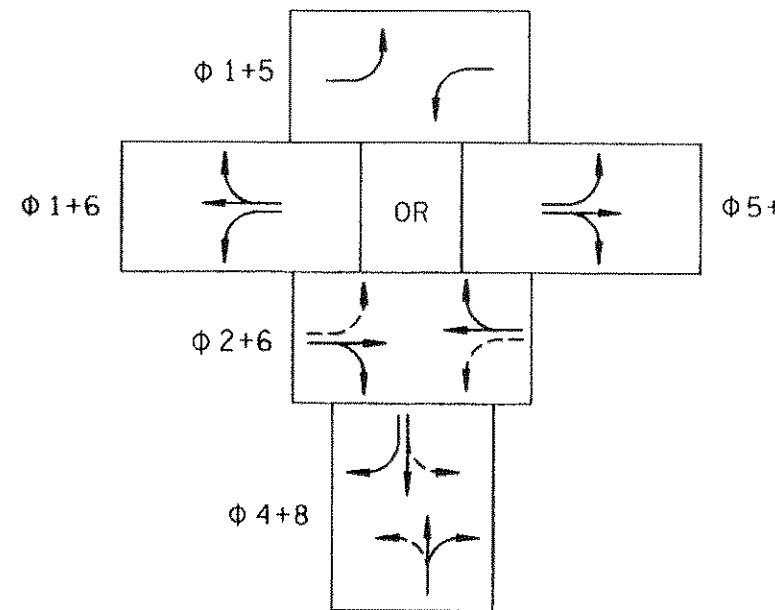
1. PROGRAM 'PASSAGE SEQUENTIAL' AS INACTIVE (N).
2. PROGRAM 'OMIT PHASE 1 BY PHASE 2 ON' AND 'OMIT PHASE 5 BY PHASE 6 ON'.
3. OTHER REQUIRED SETTINGS AND FUNCTIONS SHALL BE APPROPRIATE VALUES AS RECOMMENDED BY THE CONTROLLER MANUFACTURER.



## CONTROLLER RING DIAGRAM



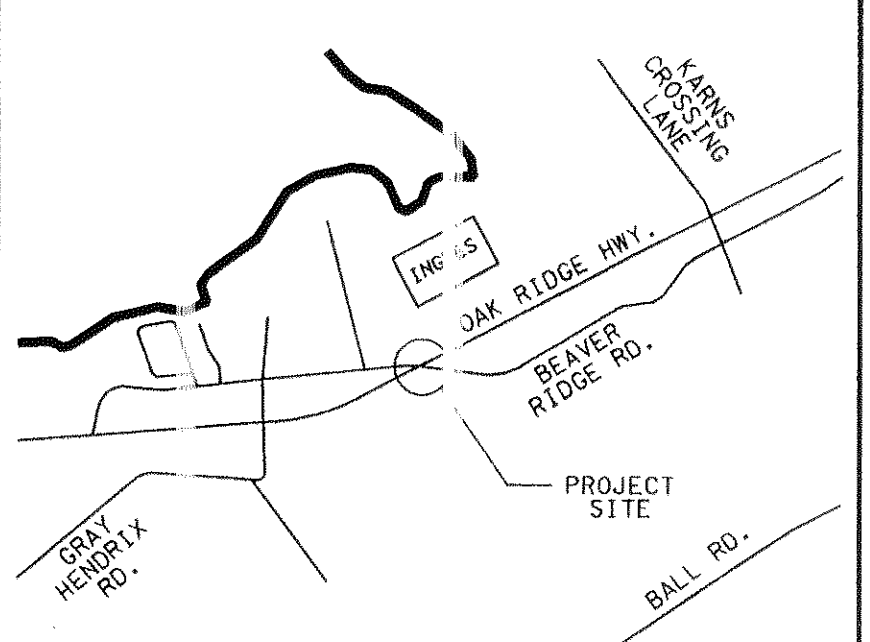
PHASING DIAGRAM



## NOTES

1. PERMITTED, BUT NOT PROTECTED VEHICULAR MOVEMENT.
2. ALL CLEARANCES SHALL BE IN ACCORDANCE WITH THE MUTCD.
3. FLASHING OPERATION: RED BALL FOR HEADS 4 AND 8; YELLOW BALL FOR HEADS 5/2, 1/6, 2, AND 6.








LOCATION MAP  
NOT TO SCALE



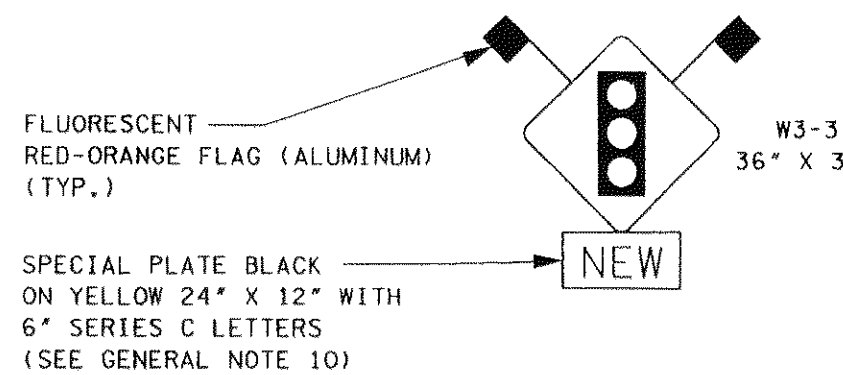
## UTILITY AND TRAFFIC CONTROL NOTES

1. THE LOCATIONS OF UTILITIES AND UNDERGROUND STRUCTURES SHOWN ON THE E PLANS ARE APPROXIMATE ONLY AND NO ALL HAVE BEEN SHOWN. THE INSTALLER SHALL COORDINATE WITH UTILITY AND PROPERTY OWNERS PRIOR TO IDENTIFY, RELOCATE, AND PROTECT FEATURES AS NECESSARY PRIOR TO EQUIPMENT INSTALLATION. SOME UTILITIES CAN BE LOCATED BY CALLING THE TENNESSEE ONE CALL SYSTEM, 1-800 351-1111.
2. THE COST OF ANY DAMAGE TO UTILITY FACILITIES SHALL BE BORNE BY THE INSTALLER.
3. INSTALLER SHALL COORDINATE WITH THE LOCAL ELECTRIC UTILITY WITH REGARD TO PROVIDING ELECTRICAL SERVICE CONNECTION.
4. TEMPORARY TRAFFIC CONTROL DEVICES AND MEASURES SHALL BE PROVIDED DURING CONSTRUCTION IN FULL COMPLIANCE WITH THE LATEST EDITION OF "THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
5. TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERRECTED UNLESS RELATD CONDITIONS ARE PRESENT NECESSITATING CHANGING.

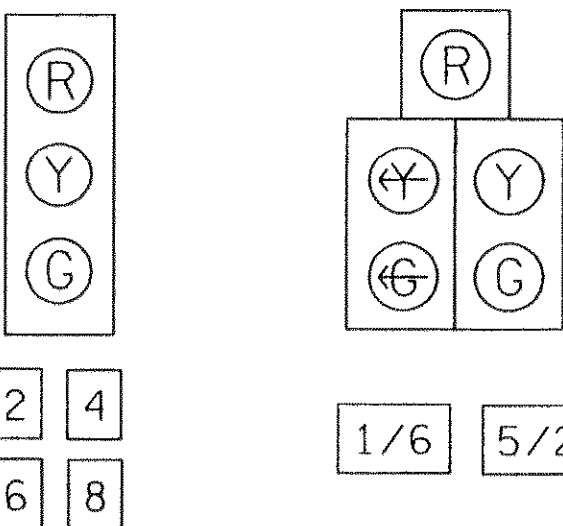
### SIGNALIZATION LEGEND

- |   |  |
|---|--|
|  | SIGNAL SUPPORT POLE (STEEL)                            |
|  | SIGNAL HEAD WITH BACKPLATE<br>AND ASSOCIATED PHASE     |
|  | PAD MOUNT CABINET AND CONTROLLER                       |
|  | STOP BAR LOOP DETECTOR WITH<br>LEAD-IN AND LOOP NUMBER |
|  | PULLBOX  |
|  | UNDERGROUND CONDUIT (SIZE)                             |
|  | EMERGENCY VEHICLE PREEMPT<br>DETECTOR                  |

## TEMPORARY SIGN DETAILS



## SIGNAL HEAD DETAILS



ALL TRAFFIC SIGNAL HEADS SHALL BE 12-INCH WITH  
BLACK HOUSINGS AND 5-INCH LOUVERED BACKPLATES.  
DISPLAYS SHALL BE L.E.D. TYPE.

## SPAN AND POLE DATA

SPAN	POINT OF ATTACHMENT	POLE	STATION	OFFSET	FOOTING DEPTH
A-B	29'	A	9+50.00	46' RT.	SEE NOTE 4
B-C	29'	B	9+70.00	53' LT.	SEE NOTE 4
C-D	29'	C	10+50.00	44' LT.	SEE NOTE 4
D-A	29'	D	10+43.00	46' RT.	SEE NOTE 4

## NOTES

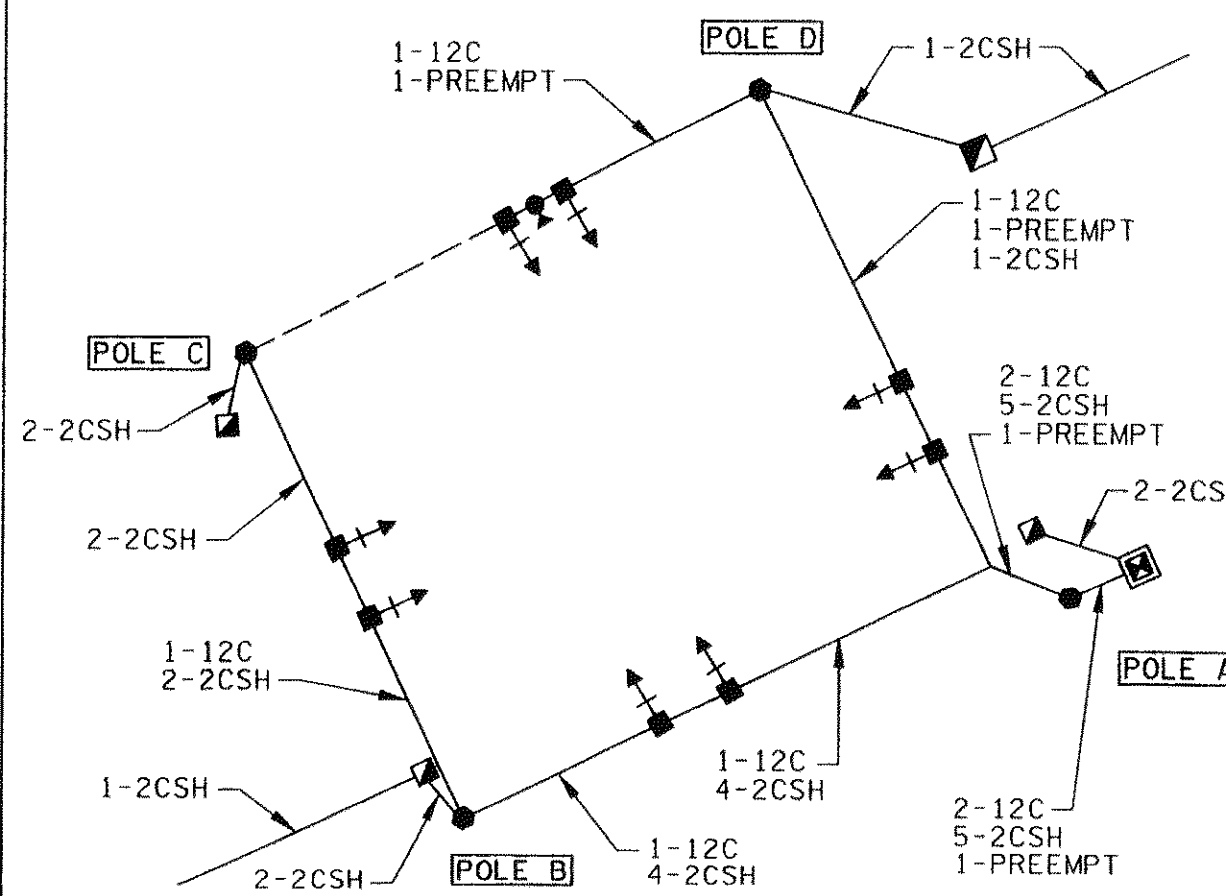
1. POINT OF ATTACHMENT IS ELEVATION DIFFERENCE BETWEEN SPAN TIE-IN POINT OF POLE AND MAXIMUM PAVEMENT ELEVATION UNDER SPAN.
2. ACTUAL POLE PLACEMENT LOCATION MAY VARY FROM THAT SHOWN ON THESE PLANS THEREFORE SHAFT LENGTH AND LOCATION OF SPAN TIE-IN POINT MAY ALSO VARY DEPENDING ON THE GROUND ELEVATION AT THE ACTUAL POLE PLACEMENT LOCATION THE CONTRACTOR SHALL DETERMINE GROUND LINE ELEVATIONS, REQUIRED SHAFT LENGTHS, AND REQUIRED LOCATIONS OF SPAN TIE-IN POINT PRIOR TO ORDERING POLES.
3. STATION AND OFFSET ARE BASED ON THE PROPOSED CENTERLINE OF BEAVER RIDGE ROAD.
4. SPAN WIRE SUPPORT CABLE SHALL BE MINIMUM 3/4 INCH DIAMETER AND SHALL PROVIDE MINIMUM BREAKING STRENGTH OF 10,000 LBS.
5. FINAL DESIGN OF POLES AND FOUNDATIONS ARE THE RESPONSIBILITY OF THE POLE MANUFACTURER AND CONTRACTOR.

## DETECTOR LOOP SCHEDULE

UNIT NO.	CONNECTED TO LOOP	LOOP SIZE	WIRE RUNS	CONNECTED TO PHASE	DETECTOR OUTPUT	DELAY SETTING
1	1-1	6' X 50'	2	1	DELAY/NORMAL	3.0 SEC
2	2-1, 2-2	6' X 6'	4	2	NORMAL	-
3	4-1	6' X 50'	2	4	DELAY/NORMAL	5.0 SEC
4	5-1	6' X 50'	2	5	DELAY/NORMAL	3.0 SEC
5	6-1, 6-2	6' X 6'	4	6	NORMAL	-
6	8-1	6' X 50'	2	8	DELAY/NORMAL	10.0 SEC
7	8-2	6' X 50'	2	8	DELAY/NORMAL	3.0 SEC

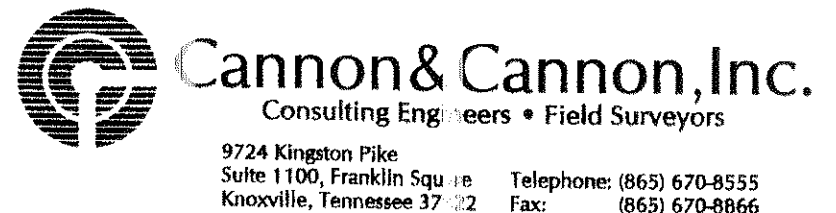
1. DETECTOR UNITS WITH OUTPUT SPECIFIED AS DELAY/NORMAL SHALL HAVE A TIMED DELAYED OUTPUT WHEN ASSOCIATED PHASE NOT GREEN, DELAY SETTINGS AS SPECIFIED. OUTPUT SHALL BE NORMAL WHEN ASSOCIATED PHASE IS GREEN.
2. ALL DETECTOR UNITS SHALL OPERATE IN CONTINUOUS-PRESENCE MODE, WITH MEMORY PROGRAMMED AS "OFF" (INACTIVE).
3. ALL 6'X50' LOOPPS SHALL BE QUADRIPOLE TYPE.

## WIRING DIAGRAM



EACH SIGNAL HEAD ASSEMBLY SHALL BE SUPPLIED WITH A  
12-POSITION QUICK DISCONNECT HANGER WIRED AS FOLLOWS

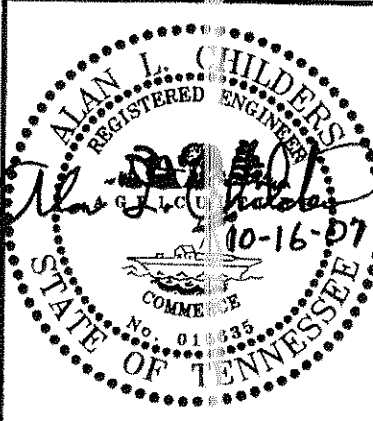
POSITION	CONDUCTOR COLOR	PHASE INDICATION
1	GREEN	#2 OR #6 GREEN BALL
2	ORANGE	#2 OR #5 YELLOW BALL
3	RED	#2 OR #6 RED BALL
4	BLUE	#4 OR #8 GREEN BALL OR SPARE
5	WHITE/BLACK	#4 OR #8 YELLOW BALL OR SPARE
6	BLACK	#4 OR #8 RED BALL OR SPARE
7	GREEN/BLACK	SPARE
8	ORANGE/BLACK	SPARE
9	RED/BLACK	SPARE
10	BLUE/BLACK	#1 OR #5 GREEN ARROW OR SPARE
11	BLACK/WHITE	#1 OR #5 YELLOW ARROW OR SPARE
12	WHITE	#1 OR #5 NEUTRAL



CLIENT:  
INGLES MARKETS, INC.  
P.O. BOX 6676  
ASHEVILLE, NORTH CAROLINA 28816

PROJECT:  
REALIGNMENT OF BEAVER RIDGE ROAD  
AT OAK RIDGE HIGHWAY  
KNOX COUNTY, TENNESSEE

## TRAFFIC SIGNAL PLAN



CCI PROJECT NO.	318-0002
DATE	10-15-07
P.I.C.	ALC
DRAWN	JCE
Q.C.	ALC

8.0